

25th Avenue

Along with the Shopping Center, the 25th Avenue business district is one of the Station Area's major neighborhood-serving commercial districts. The street is lined with small businesses in one- to two-story well maintained and attractive buildings. Pedestrian-scaled storefronts, including good ground floor transparency and attractive signing and awnings, add to the identity of a thriving local district. An improved public realm will complete and enhance the neighborhood's identity as a pedestrian-oriented shopping district.

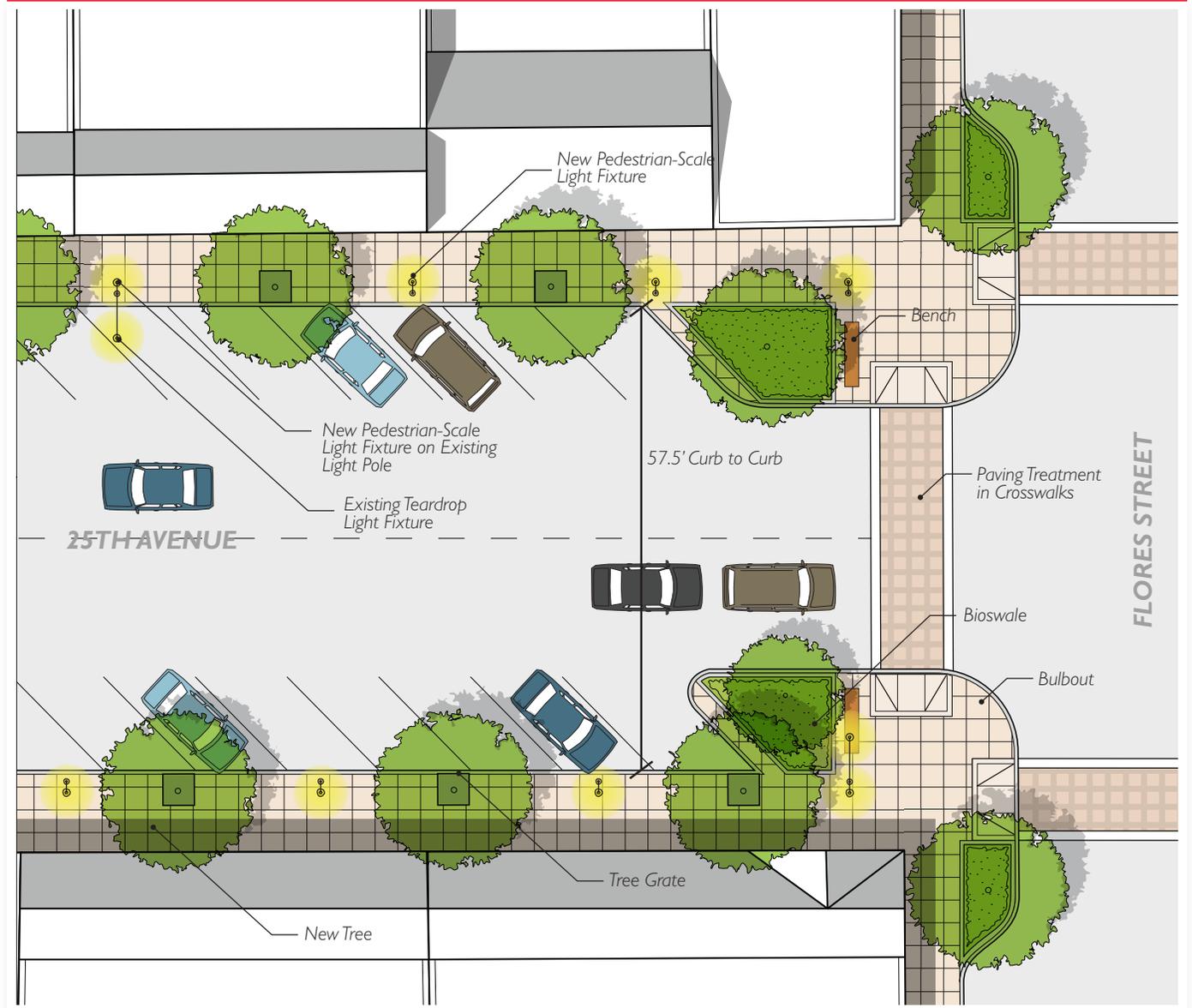
The curb-to-curb width, the width of the roadway including travel lanes and parking stalls, can be narrowed to a minimum standard to gain potential right-of-way for additional sidewalk width, retaining the throughway zone while providing additional space for trees to be planted in the furnishing zone. This width change is illustrated in Figure 5-10. To further enhance the throughway zone for pedestrians and effectively widen sidewalk width even more at certain portions of the street, bulb-outs, enhanced crosswalks, and vegetation-based stormwater treatment systems are proposed at key intersections and at the midblock crossing between Hacienda and Flores Streets. New pedestrian-scaled light fixtures, including installation of new light fixtures onto existing light posts, benches, and other pedestrian amenities round out the streetscape improvements on 25th Avenue to cohesively tie the district together to the greater Station Area.

Grade-Separated Railway Crossings

New grade-separated railway crossings at 25th, 28th, and 31st Avenues will connect the Station Area with Bay Meadows, Highway 101, and Saratoga Drive. They are important components that should be conceptually incorporated into the overall network of improved streets in the Station Area.

Measures should be taken in order for the underpasses to feel like they are part of the greater pedestrian network, and to avoid feeling like unsafe, desolate areas. Natural light into the underpasses should be maximized through an open design. Opportunities for public art such as murals should be explored.

Figure 5-10: Potential 25th Avenue



A. Station Area Sidewalks

1. Streetscape Standards

Edge Zone

The edge zone should create a clear end to the vehicle realm and transition to the pedestrian realm.

Furnishing Zone

The furnishing zone should be a minimum of 3 feet in width, including space for street trees, street furniture, and lights. To the extent possible, street furniture such as newspaper racks should be grouped to keep space clear for pedestrian travel.

Throughway Zone

The throughway zone should be a minimum of 5 feet in width and free of all obstructions to meet ADA requirements for pedestrian travel. Where sidewalks are wider than 8 feet total, it should be a minimum of 50% of the total sidewalk width, or 5 feet, whichever is greater.

Frontage Zone

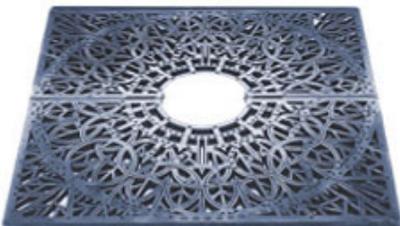
The frontage zone should be used by businesses to engage passers-by and provide items of visual interest. The frontage zone should be a minimum of 2 feet in width.

Street Furniture

- Street furniture should be provided along El Camino Real, including, but not limited to benches, trash receptacles, and newspaper racks. These should meet the following specifications:
- Benches: Wabash Valley Estate Series, Estate Benches, Slat (S) Black powder coat
- Planters: Wabash Valley Rod (O)
- Table: Designer Series Diamond (D) 30" square table
- Inverted U shape bike racks: Secure Site Designs LLC by Victor Stanley Model BRWS-101 Black powdercoat
- Tree Grate: Urban Accessories OT-Title 24, Powdercoat RAL 6004 (Black), Customized to provide two 4" holes for bubbler access
- Trash Cans: Victor Stanley Inc, Ironsites Series, S-424, 36 Gallon Capacity, Surface Mounted, Black, Powdercoated.
- Bus shelters along El Camino Real should be placed as close as possible to signalized intersections, with a focus on locations that will smooth the flow of traffic.



Inverted U shaped bike rack.



Tree grate.

- Bus shelters are desirable at all bus stops on El Camino Real, provided there is adequate space for them while maintaining ADA accessibility requirements. The City should work with SamTrans to pursue installation of new shelters, or upgrades to existing dated shelters, for all bus stops in the Station Area.
- Pedestrian amenities must be strategically located to ensure ADA compliance and a clear path of travel for pedestrians.



36 gallon capacity trash can.

B. El Camino Real

1. Development Guidelines

These guidelines apply to properties fronting El Camino Real, including the side of those parcels that fronts onto side streets at intersections with El Camino Real. Guidelines for parcels in the Transit Center area are discussed later in this chapter.

Ground Floor Design

Ground floors of buildings within 100 feet of and fronting on El Camino Real shall include building transparency of at least 50% of the building's street frontage and shall include the primary building entrance. Properties on the west side of El Camino Real that are part of the Hillsdale Shopping Center may include store display windows in calculating 50% transparency and are encouraged to have secondary entrances on El Camino Real. Where residential uses are allowed on El Camino Real, they are exempt from this guideline.

Residential uses, where allowed on ground floors of buildings facing El Camino Real, should include design features ensuring a livable environment for residents. These should include raising floor levels above sidewalk level and providing interior courtyards.

In no case shall blank walls be permitted on buildings facing El Camino Real.

Setbacks

Buildings shall be set back from El Camino Real to create a 10 foot effective sidewalk wide enough for a pleasant pedestrian environment. This shall be established per the requirements of the City's Zoning Code and shall be consistently applied throughout the Station Area.



Active ground floor uses add vibrancy.

Parking Lots

- Surface parking associated with new development shall not be located within 25 feet of the El Camino Real-facing property line for parcels, existing or combined, with 200 feet or less of El Camino Real Frontage.
- Parking lot landscaping and screening shall be provided in compliance with the City's landscape ordinance.
- Surface parking area associated with new development shall not occupy more than 25% of a property frontage within 25 feet of the property line along El Camino Real for parcels, existing or combined, with more than 200 feet of El Camino Real frontage. Existing surface parking may be retained in conjunction with moderate improvements to existing development.
- New development shall limit the number of new curb cuts onto El Camino Real, including shared access where feasible, side street access, and use of existing curb cuts.

Pedestrian Paseos

- Mixed-use and residential development on parcels more than 300 feet deep on the east side of El Camino Real shall provide a north-west pedestrian path at least 20 feet wide. The pathway shall have lighting consistent with pedestrian-scale lighting proposed on El Camino Real, and include pedestrian amenities similar to those found on public sidewalks.
- Mixed-use development flanked by El Camino Real and Palm Place is strongly encouraged to include additional entrances, awnings, and outdoor seating areas oriented along Palm Place.
- Retail and restaurant uses are strongly encouraged to have additional entrances, awnings, and outdoor seating areas facing pedestrian paseos to enhance the safety, character, and economic vitality of the Station Area.



A building with stepbacks.

Stepbacks

- Buildings taller than three stories are strongly encouraged to step back at least 6 feet above the third floor.
- Within that stepback, balconies are permitted and encouraged.

2. El Camino Real Streetscape Standards

Median

- El Camino Real shall include a median at least 12 feet wide from 25th Avenue to 31st Avenue. Where there is a turn lane, the median should be at least 4 feet wide.

- The ROW of El Camino Real fluctuates between 99 and 108 feet from 25th to 31st Avenues. In cases where the ROW is less than 100 feet, a 1-foot reduction in median width is encouraged. In cases where the ROW exceeds 100 feet, up to 4 feet of extra ROW is encouraged to be applied to the median, and the remainder to the shoulder lane.
- The median shall be landscaped and planted with appropriate vegetation, including tree species recommended by the City's Street Tree Master Plan.

Pedestrian Refuge Islands

- At all signalized crossings of El Camino Real, there shall be pedestrian refuges to provide a space for pedestrians who cannot cross the entire street during a single traffic signal sequence to wait. At most intersections, they should be a minimum of 6 feet wide.
- At 25th Avenue, a pedestrian refuge should be located only on the south side of the street. If the ROW is not adequate, the pedestrian refuge may be narrower.

Lane Widths

- The outside travel lanes on either side of El Camino Real should be narrowed to allow for the landscaped median from 25th to 31st Avenues. The lane including the shoulder should be a maximum of 15 feet wide which is wide enough for bicycles to ride adjacent to traffic.

Street Trees

- Street trees shall be planted along all the entirety of El Camino Real in the furnishing zone at consistent intervals of 20 to 30 feet.
- Sidewalk street tree species should be London Plane tree, consistent with the El Camino Real Master Plan.
- Trees should be planted in City standard tree grates that are ADA compliant, such that the grates do not present obstacles for persons with mobility impediments.

Street Lights

- Pedestrian-scale light fixtures recommended by the El Camino Real Master Plan should be installed all along the El Camino Real corridor within the Station Area.
- Light fixtures should match the Teardrop Series as recommended in the ECRMP, and be installed on pedestrian-scaled light poles no taller than 15 feet. Pedestrian-scale light fixtures should be installed on existing light poles.



A street with trees planted at a consistent interval

Theme Intersections As proposed by the El Camino Real Master Plan and supported by the Rail Corridor Plan, intersections at 25th and 31st Avenues should be redesigned to include:

- Enhanced paving treatment in crosswalks and pedestrian refuge islands.
- New streetlights with pedestrian fixtures.

The 31st Avenue intersection should include:

- 6-foot medians to provide a median pedestrian refuge space for pedestrians who cannot cross the entire street during a single traffic signal sequence to wait.
- A monument sign within the median on the north side of the intersection, pursuant to Caltrans approval. The monument sign should reference the Hillsdale Caltrain Station and Transit Center.
- Canary Island Date Palm (*Phoenix canariensis*) at all four corners (existing).

C. East-West Avenues

1. Development Guidelines

The guidelines in this section apply to properties fronting 28th and 31st Avenues, except for the Transit Center area and the Hillsdale Shopping Center property, which are discussed later in this chapter.

Setbacks

- Buildings shall be set back to create a 10 foot effective sidewalk wide enough for a pleasant pedestrian environment, plus any additional ROW for bicycle facilities.
- Areas adjacent to entries and display windows should be predominantly hard-scape. Parking shall not be allowed in front setback areas.
- Buildings are strongly encouraged to set portions of the building back to create usable outdoor space. Planters and well-designed, low-sitting fences should be used to define the edge of the outdoor space. Parking shall not be allowed in front setback areas.
- Corner properties with El Camino Real frontage shall provide setbacks on 28th and 31st Avenues, consistent with the El Camino Real Master Plan.

Parking Lots

- Surface parking area is strongly encouraged to be located in the rear of the building, and should be avoided along the front property line.

2. Streetscape Standards

Street Trees

- Street trees shall be planted along both streets in the Station Area at consistent intervals of 20 to 30 feet.
- In order to complement El Camino Real and create a cohesive character for the Station Area, tree species should be the species recommended by the City's Street Tree Master Plan.
- Trees should be planted in grates that are ADA compliant, such that the grates do not present any obstacle for persons with mobility impediments.
- 28th Avenue street trees should be made Maidenhair Ginkgo trees (*Ginkgo biloba*) on the east of El Camino Real to the train tracks. West of El Camino Real, the City's Street Tree Master Plan should be consulted.
- These standards do not apply to streets internal to the Hillsdale Shopping Center. Landscaping improvements on these streets should be part of a landscape master plan for the Shopping Center.

Street Furniture and Street Lights

- Street furniture and street lights shall be provided as described for the Station Area in A. Station Area Sidewalks.
- Pedestrian amenities must be strategically located to ensure ADA compliance and a clear path of travel for pedestrians.

D. Transit Center

1. General Principles

- The primary function of the Transit Center is to provide rider access to the relocated Caltrain station and boarding platforms located between the extensions of 28th and 31st Avenues to El Camino Real.
- The station is designed to facilitate the transfer of people once they arrive at the station to the train platforms, regardless of how they access the station.
- The Transit Center is intermodal in that it provides convenient and integrated connectivity between modes and services at the center. The center is designed to accommodate conventional buses, future Bus Rapid Transit, employer and/or privately operated shuttles, taxis, bicycles, and pedestrians. It anticipates potential conflicts between these users and is designed to minimize them. Travelers arriving by private automobile may be dropped off or picked up at kiss-and-ride loading curbs or park within the station's parking structures.

- Mixed-used development in and around the Transit Center is important to the center's success. This type of development creates transportation efficiency through proximity to public transportation, opportunities for shared parking, and provision of everyday needs, such as retail and eating establishments, to Transit Center users.

The Transit Center will benefit from train tracks on an aerial viaduct structure. It will enhance visual, bicycle, and pedestrian connectivity with Bay Meadows II, as well as provide space that can be devoted to Transit Center circulation, parking, and amenities.

2. Development Guidelines

The guidelines in this section apply to the parcels bounded by the Peninsula Station development and 31st Avenue to the north and south, and El Camino Real and the Caltrain ROW to the east and west.

Parking Garage

- A parking garage servicing Caltrain users, convenience retail, and other nearby activity should be located along El Camino Real near midblock.
- The garage shall provide ADA compliant parking spaces and paths of travel for persons with mobility impediments.
- If the parking garage is a standalone project, retail shall occupy the ground-floor space along the frontage of El Camino Real, at the minimum depth of 40 feet. The ground floor may be dedicated to parking if the garage is part of a larger, mixed-use project.
- The garage shall be designed to fit into contextual development, be human-scaled, and follow the El Camino Real Master Plan guidelines to the extent possible.
- Real-time parking space availability signage shall be included in the parking garage.

Signage

- Signage within the Transit Center and guiding patrons to the Transit Center shall be created consistent with the Station Integration Plan and be developed in collaboration with neighboring property owners such as Bay Meadows Phase II.

Intermodal Circulation

- An off-street bus turn-around with adequate turning radius and bus-stop space is strongly encouraged.
- A lane or section dedicated to kiss-and-ride travelers shall be provided, and should be located as close to the Caltrain platforms as possible.

Caltrain Station Building

- A well-designed station building shall be developed and should occupy a prominent location, visible from the rest of the Transit Center. It should be a focal point for the Transit Center, define arrival at Hillsdale Station, and establish a strong sense of place.
- The station building is strongly encouraged to include convenience retail uses or a cafe to accommodate travelers and provide a sense of activity. In addition, the station building should provide ticket-sale or similar services, information about train arrival and departure times, and pedestrian, bicycle, bus, and shuttle connection options.

TOD Mixed-Use Project

- A mixed-use project with residential and/or office uses above the first floor should be developed to fully take advantage of the proximity of the station.
- The project should be designed to be simultaneously oriented to El Camino Real, the Transit Center, and the Caltrain right-of-way so that all faces of the building have a strong contextual relationship to its surroundings.

Plazas

- There should be a public plaza, or combination of plazas, of at least 20,000 square feet for use by short-term parking/pick-ups. The space should be flexible to include public gatherings.
- The plaza should be mostly hardscape, with minimal and hardy landscaping so that it can host various activities and gatherings.
- Public art should be provided in the plaza, consistent with the City's Public Art Ordinance.

Setbacks

- Buildings shall be set back from the property line to create 20 feet of effective sidewalk width along El Camino Real, inclusive of the existing width of the public sidewalk. The extra sidewalk width is intended to provide for ease of mobility for bus users and to signify the Transit Center as a unique destination within the Station Area.
- Buildings, not including the garage, are strongly encouraged to set portions of the building back from El Camino Real to create usable outdoor space. Planters and well-designed, low-sitting fences should be used to define the edge of the outdoor space. Parking shall not be allowed in front setback areas.

- Buildings adjacent to the train station platform shall include a 10-foot rear yard setback, which shall be designed to provide bicycle and pedestrian access to the train station platform, including stairs or other structures as appropriate to the height of the tracks and configuration of the building.

Bicycle Amenities

- Bicycle parking facilities should be provided in a safe, well-lit, and convenient location near the station building.
- The number and types of facilities should correspond to the recommendations in the Caltrain Bicycle Access and Parking Plan to the extent possible, and modified to fit the new station location, as detailed in Appendix A.
- Stairways should incorporate bicycle channels for easier access to and from the platforms; however, if the platform is low enough to allow ramp access, that is preferred over channels due to operational issues associated with channels

Universal Access

- Pavement should be designed for accessibility by all users. Pavement should be stable, firm, and slip resistant, should contrast with walls and adjacent buildings, and should not have strong, confusing patterns, or textures with changes in direction.
- Paths of travel should be clear and placement of lights, benches, newsstands, and other pedestrian amenities should be placed out of the path of travel.

Safety and Crime Prevention

Crime Prevention through Environmental Design (CPTED) is a crime prevention philosophy based on the theory that the proper design and effective use of the built environment can lead to a reduction in the fear and incidence of crime, as well as an improvement in the quality of life. CPTED emphasizes understanding and changing the physical environment in an effort to reduce crime at particular locations. Using the principles of CPTED, this Plan recommends the following approaches to safety and crime prevention:

- Natural surveillance shall be used in the design of buildings to maximize visibility of spaces throughout the Transit Center. This shall include windows or open areas to create visual connections to the interior of the parking garage and ground floor uses of the station building and TOD mixed-use project. Windows allow pedestrians and motorists passing by to see into the parking area and detect criminal activity. In the event that a crime does occur, there is a greater chance that it will be seen and reported to police.

- Landscaping shall be properly trimmed and maintained to allow visibility.
- Lighting shall be pedestrian-scale, highlighting the pedestrian environment.
- Signage should clearly demarcate the transition between public, private, and semi-private spaces. The design of this signage should be coordinated with the Station Integration Plan.
- Small decorative fencing should be placed around the semi-private outdoor patio of a business.
- Well marked pedestrian pathways should be created through parking lots, which give direction to its users, and create a safer path of travel by alerting drivers to the concentrated presence of pedestrians.
- To the extent possible, non-motorized paths of travel, including bicycle and pedestrian, should be clearly separated from vehicular traffic.
- Additional measures as required by the City's Building Security Code.

3. Streetscape Standards

Mid-block Signalized Crossing

- A signalized southbound left turn and related crosswalks shall be installed near the midpoint between 28th and 31st Avenues, subject to traffic studies identifying traffic impacts. The intersection and crosswalks shall provide for enhanced vehicular, transit, and pedestrian circulation in relationship with the garage and Intermodal Transit Center.
- The southbound left turn lane should lead directly into the new parking garage or Transit Center.
- Pedestrian safety features should be incorporated into the design of this crossing.

Street Furniture

- Street furniture should be provided, including, but not limited to benches, trash receptacles, and newspaper racks.
- Amenities must be strategically located to ensure ADA compliance and a clear path of travel for pedestrians.

E. Key Intersections

The guidelines in this section apply to the parcels that surround the intersections of 28th and 31st Avenues and Hillsdale Boulevard at El Camino Real. Setbacks for urban open space are encouraged, and guidelines for these open spaces are described in Section 1 below. Where corner open space proves difficult to achieve due to site constraints, corner building features should be provided at these intersections, subject to guidelines in Section 2 below.



Buildings set back to create active open space.

1. Development Guidelines for Urban Open Space

Where such regulations apply, corner open spaces will count toward on-site open space requirements under the City's Zoning Code.

Setbacks

Buildings are strongly encouraged to provide additional setback to create urban open spaces. These should be consistent at the four corners of an intersection; would remain part of private development; and would not be publicly dedicated:

- El Camino Real: 30 feet beyond that described in Section B; 10 feet beyond for buildings that are two stories and/or 28 feet in height or less.
- East-West Avenues: 20 feet beyond that described in Section C; 10 feet beyond for buildings that are two stories and/or 28 feet in height or less.

Stepbacks

- Stepbacks are not required, and are strongly discouraged, for the portions of the building that face any open space developed at key intersections.

Open Space Design

- Active ground-floor uses are strongly encouraged to have entrances, awnings, and outdoor seating area in the urban open space.
- At least 50% of the open space should be treated as a plaza, with hardscape, benches, tables, pedestrian amenities, and lighting.
- At least 25% of the open space should be treated with sustainable stormwater features. This could be a central water feature such as a rain garden, or a number of features, such as swales.
- Public art should be provided in the urban open space, consistent with the City's Public Art Ordinance.
- If right-of-way space allows, use bulbouts at corners containing urban open space to expand the overall size of the open space and increase the buffer distance between traffic and open space users.



Development oriented to open space.

2. Development Guidelines for Corner Building Features

Corner Building Features

- As an alternative to open space setbacks, buildings located at key intersections shall be oriented and architecturally designed to emphasize the corner as a node of activity and architectural prominence. Solutions for developing projects that are of an exemplary quality include:
- Tower elements as a prominent massing feature
- Entry plazas on corner sites
- Fountains or water features
- Distinct changes in the building volume at the primary entry
- Prominent landscape features, such as tall trees
- Dramatic building lighting for nighttime effect
- Public art installations

The primary building entry should be oriented towards the corner and be visible from the intersection.

Stepbacks

- Stepbacks are not required, and are discouraged, for the portions of the building within 40 feet of the corner at key intersections.

F. Laurel Creek

1. Development Guidelines

The guidelines in this section apply to the parcels that are adjacent to Laurel Creek.

Setback

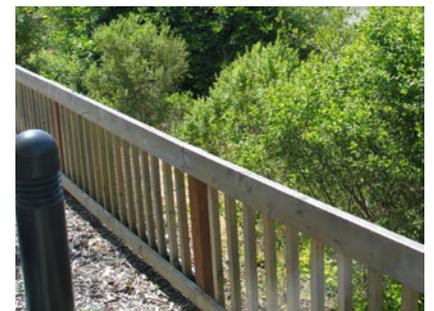
- Buildings shall be set back from Laurel Creek consistent with the City’s General Plan and zoning regulations.
- This setback should be designed as a passive park-like setting, which could include trails, and be publicly accessible.

Fencing

- No solid fencing should be installed along the creek. This will ensure an open and safe environment. If fencing is required, the fence should be designed with visibility in mind, use high-quality materials, and be a maximum of 4 feet in height.



A tower element makes the corner of the building stand out.



An attractive low-lying fence along a creek.



An example of a small creek sign.

Safety

- Development should orient windows and entries to face the creek to provide “natural surveillance” on the creek throughout the day.

2. Streetscape Standards

These standards apply to El Camino Real where it is directly adjacent to the centerline of the Creek.

Signage

- In order to reinforce the identity of the creek, a monument sign highlighting the presence of Laurel Creek should be erected in a location that is clearly visible to pedestrians.
- The sign should not be so large or placed in a location that presents a visual distraction or obstruction for residents, pedestrians, and drivers.

G. Hillsdale Shopping Center

1. Development Guidelines

The guidelines in this section apply to portions of the Hillsdale Shopping Center that directly face 31st Avenue and Hillsdale Boulevard.

Setbacks

- Buildings shall be set back from El Camino Real consistent with the requirements of City’s Zoning Code and the El Camino Real Master Plan, in order to provide for a 10-foot effective sidewalk width, wide enough for a pleasant pedestrian environment.
- Buildings should be built to this effective sidewalk setback.
- Buildings are strongly encouraged to have portions set back to create usable outdoor space. Planters and well-designed, low-sitting fences should be used to define the edge of the outdoor space.

Pedestrian Bridge

- Consider modifications to the pedestrian/retail bridge over 31st Avenue to allow for better visual connections to the street below, and for more light down to street level.
- Consider installation of enhanced crosswalk treatments, such as lighted crosswalks, to increase the visibility of pedestrians crossing the street.

Buildings

- All new or remodeled buildings should be pedestrian-scale and designed to add visual interest to the street.
- Entries to retail uses should open onto the sidewalk where possible.
- In order to complement El Camino Real and create a cohesive character for the Station Area, buildings should be well-designed and follow the guidelines from the El Camino Real Master Plan. In general, blank ground floor building walls should be avoided while design elements that heighten visibility and transparency are strongly encouraged.
- New buildings are strongly encouraged to be constructed on existing surface parking areas that front the street in order to reduce the visual prominence of parking lots.
- The bookends of the existing parking structure are encouraged to be redesigned as retail uses with at least a 40-foot depth.



Pedestrian-scaled retail with entryways and windows oriented to the sidewalk.

Relationship with Laurel Creek

- In order to activate the creek along the boundary of the Shopping Center, restaurant and café uses should incorporate outdoor seating areas that face the creek and bridge.
- Consider the removal of the chain link fence that physically and visually separates the creek and the Shopping Center, or the installation of a low-lying, more visually appealing fence, as described in Section E.

2. Streetscape Standards

Please refer to the Streetscape Standards in Section C, East-West Avenues.

H. 25th Avenue

1. Streetscape Standards

Roadway

- Existing roadway width at West 25th Avenue is approximately 60 feet. Consider the narrowing of the curb-to-curb width to a maximum of 57.5 feet to allow for safe and efficient movement of vehicles as well as gaining potential right-of-way for wider sidewalks.

Sidewalks

- Convert any extra right-of-way gained from the narrowing of the street to usable sidewalk area devoted to either throughway or furnishing zone.
- Sidewalk materials should be designed to provide additional interest and create a sense of place that distinguishes 25th Avenue from the Downtown area of San Mateo.

Street Trees

- The Capital Pear (*Pyrus calleryana*) should be planted on 25th Avenue, as shown in Figure 5-10.

Street Furniture

- Street furniture and street lights shall be provided as described for the Station Area in A. Station Area Sidewalks.

Street Lights

- Street lights on 25th Avenue should be pedestrian-scaled and provided at consistent intervals of every 30 feet, following the guidelines shown for El Camino Real.

Utilities

- Pursue undergrounding of telecommunications and other overhead wiring on East 25th Avenue.



Bulbouts can extend the pedestrian realm for additional space.

Bulbouts

- Bulbouts should be installed where 25th Avenue intersects Flores and Hacienda Streets. Bulbouts should also be provided at the midblock crossing between these streets.
- Pedestrian amenities such as benches, trash cans, bicycle parking and, ganged newspaper racks should be provided in the bulbouts.
- Vegetation-based stormwater treatment systems should be installed in the bulbouts to provide a decorative corner treatment and aid in stormwater management.

Crosswalks

- In order to complement El Camino Real and create a cohesive character for the Station Area, crosswalks should have enhanced pavement features that are similar to that of the theme intersections.
- Upgrade the mid-block crossing between Hacienda Street and Flores Street to improve pedestrian visibility using approaches including lights and a raised crosswalk.

I. Grade-Separated Railway Crossings

Streetscape Standards

- At grade-separated roadways, sidewalks should be separated from traffic and located on top of retaining walls so that the pedestrian pathway is as evenly graded as possible.
- Pedestrian and bicycle access to the Transit Center should be provided from the 31st Avenue crossing immediately on the west side of the train tracks.
- Additional traffic safety features, such as well-designed fencing, should be incorporated into the overall design of the underpasses.
- Consider high-quality, well-designed fencing as a safety and design feature along the sidewalk. Public art should be considered in the design of the fencing.
- If an underpass abuts an existing or proposed building, clear pedestrian access should be available to the property.
- Retaining walls below the train tracks should be treated with design elements, including public art, to create interest and increase visibility and safety of pedestrians.
- The underpasses shall be well-lit with fixtures that direct a clear path of travel.
- If possible, overhead track structures should be separated to allow natural light underneath.



Attractive and safe underpass design features.

5 Urban Design